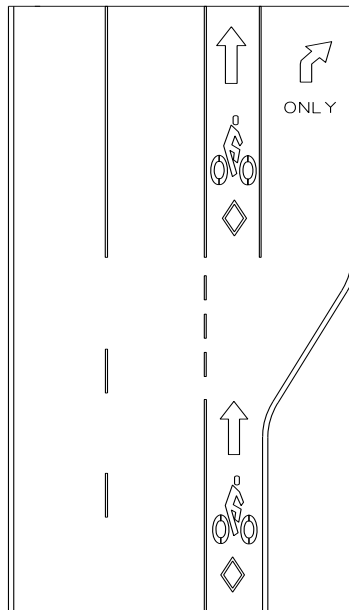


- Bike Lanes and Turning Lanes

Bike lanes complicate bicycle and motor vehicle turning movements at intersections. It is preferable to continue the same width of bike lane through the intersection. Locations where a bike lane approaches an intersection (4 feet from the edge of pavement on a curb and gutter roadway), the 4 foot wide section should continue parallel to the left of a right turn lane.



RIGHT TURN ONLY LANE

FIGURE A-5-3

- Bicycle Lanes Approaching Right-Turn-Only Lanes

NOTES: For other intersection situations see the AASHTO Guide for the Development of Bicycle Facilities. For current typical bicycle lane pavement markings see VDOT Road and Bridge Standards or current insertable sheets.

Figure A-5-3 presents a treatment for pavement markings where a bike lane approaches a motorist right-turn-only lane. The design of bike lanes should include appropriate signing at intersections to warn of conflicts. The approach shoulder width should be provided through the intersection, where feasible, to accommodate right turning bicyclists or bicyclists who prefer to use crosswalks to negotiate the intersection.