

SIGN PLACEMENT

Normal placement of warning signs in urban areas should be 250 feet minimum, in rural areas 750 feet minimum, and on high-speed roads and freeways 1500 feet minimum, in advance of a hazard or condition.

Unless otherwise specified, all vertical and horizontal spacing between signs in an assembly shall be one inch. Two signs for different purposes shall be placed not less than 800 feet apart, except in urban areas where a lesser spacing may be necessary. This general rule applies also to signs facing in opposite directions, which, to minimize distraction, should not be placed immediately opposite each other on each side of the roadway.

All signs mounted on wood posts and untapered metal posts shall be mounted at an angle of 93° between the face of the sign and the edge of the pavement. Where signs are mounted on tapered posts, the face of the sign shall make an angle of 90° with the edge of the pavement as the taper of the pole provides vertical angle that will eliminate specular glare. On curves, the angle of placement should be measured between the face of the sign and a one hundred foot chord.

PAVEMENT EDGE OR CURB TOP TO SIGN BOTTOM

RURAL: 5' minimum URBAN: 7' minimum to 10' maximum
Height to a secondary sign mounted below another sign may be 1' less.

PAVEMENT EDGE TO BOTTOM OF EXPRESSWAY SIGN

DIRECTIONAL: 7' minimum SECONDARY: 5' minimum

Route markers, warning and regulatory signs shall be at least 6' above pavement edge. However, if placed 30' or more from roadway, the sign may be 5' above pavement edge.

LATERAL PLACEMENT OF SIGNS

RURAL: Not less than 6' from edge of shoulder, or if none, 12' from edge of roadway.

URBAN: Not less than 2' from face of curb or never less than 1' when space is limited.

EXPRESSWAY: 10' minimum from edge of roadway or unmountable curb.

LARGE GUIDE SIGNS: Preferably 30' minimum, but never less than 10' from edge of roadway.

ANGLE

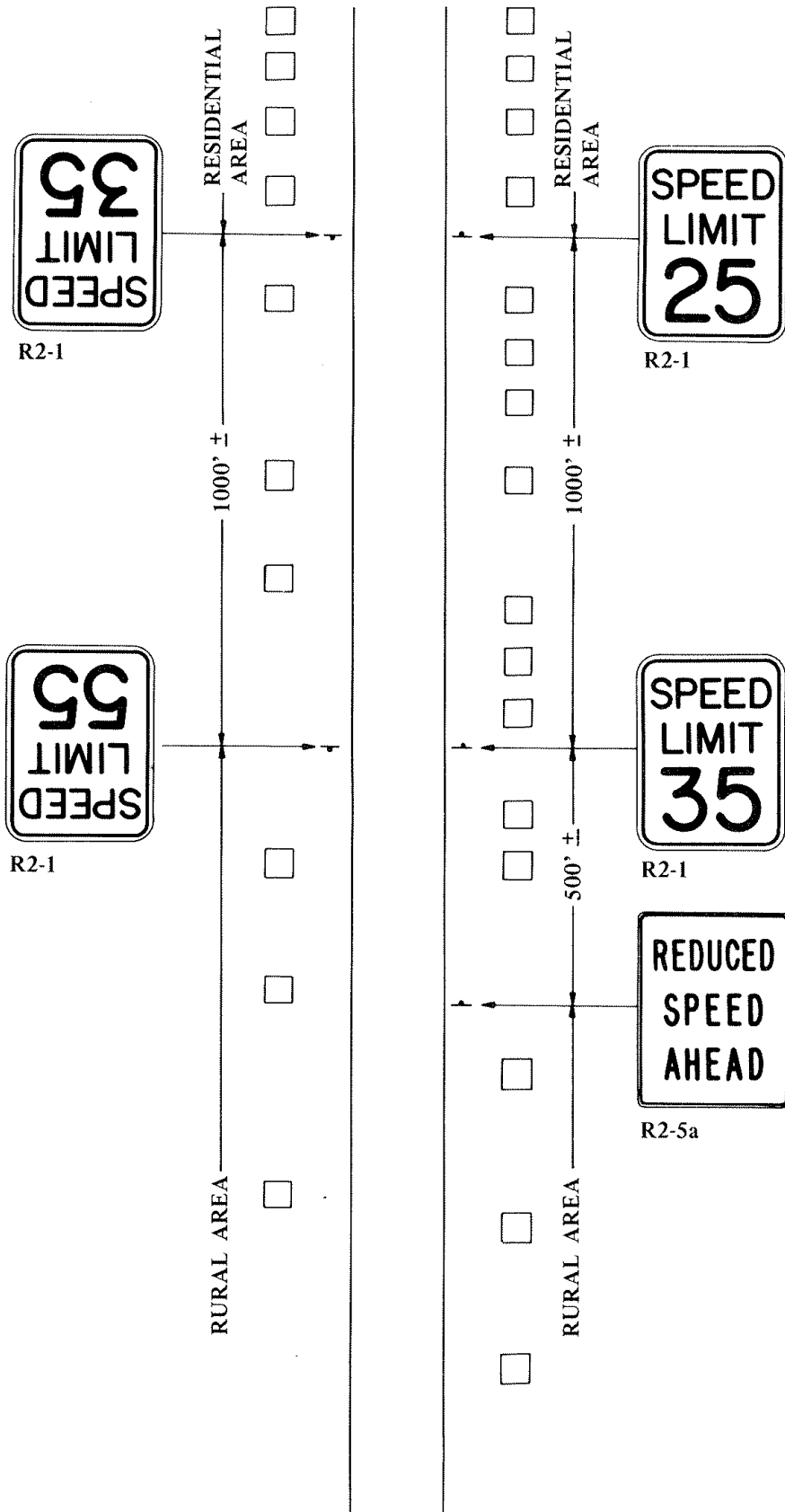
Generally, the angle of sign face to pavement edge should be 93° unless otherwise specified.

OVERHEAD SIGN PLACEMENT

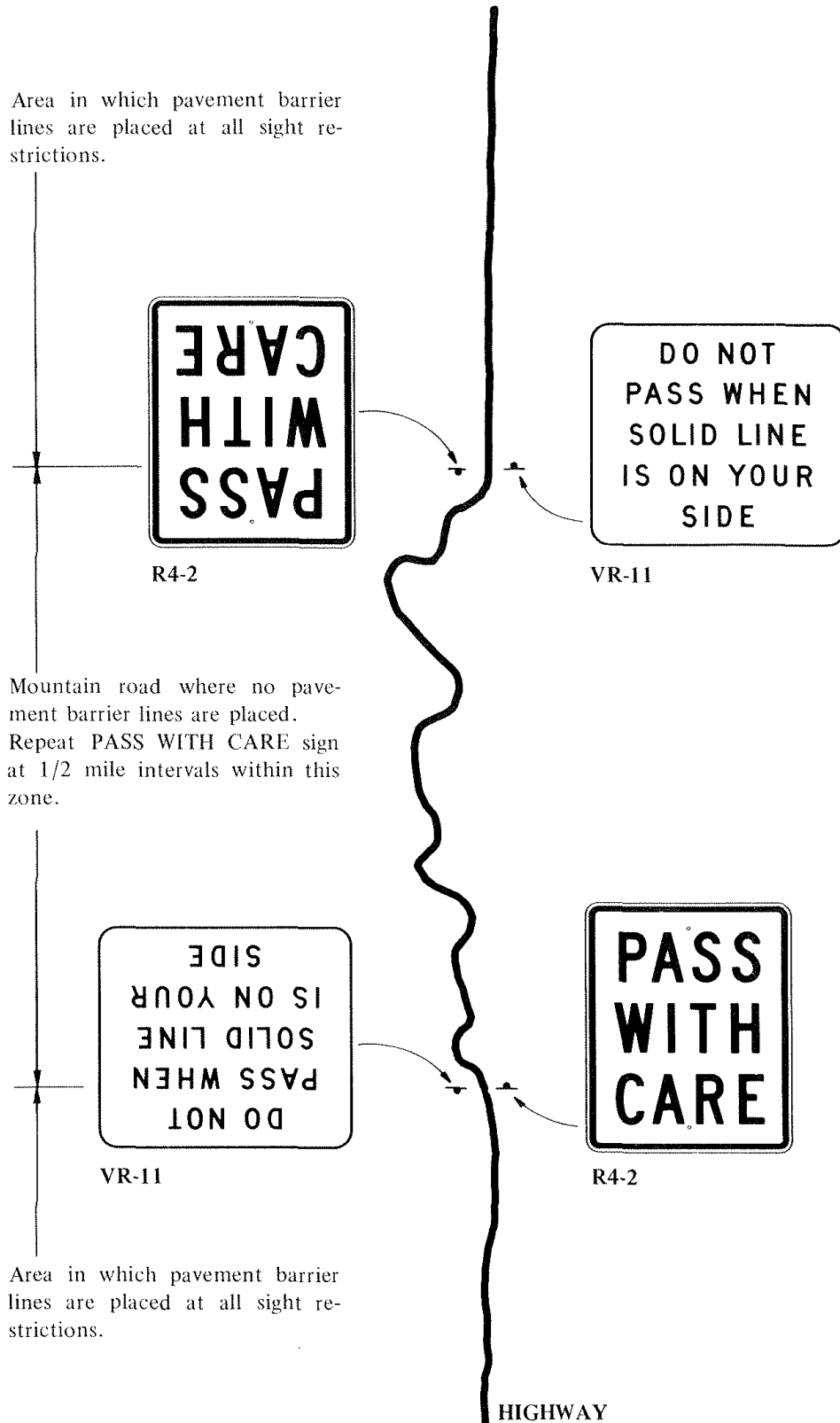
Signs mounted on cantilever or double pole supports shall have a 19' minimum clearance from highest point of roadway to sign bottom, and a minimum clearance of 17'-6" from highest point of roadway to walkway or luminaire support.

Differences from these specifications will be noted with the individual sign.

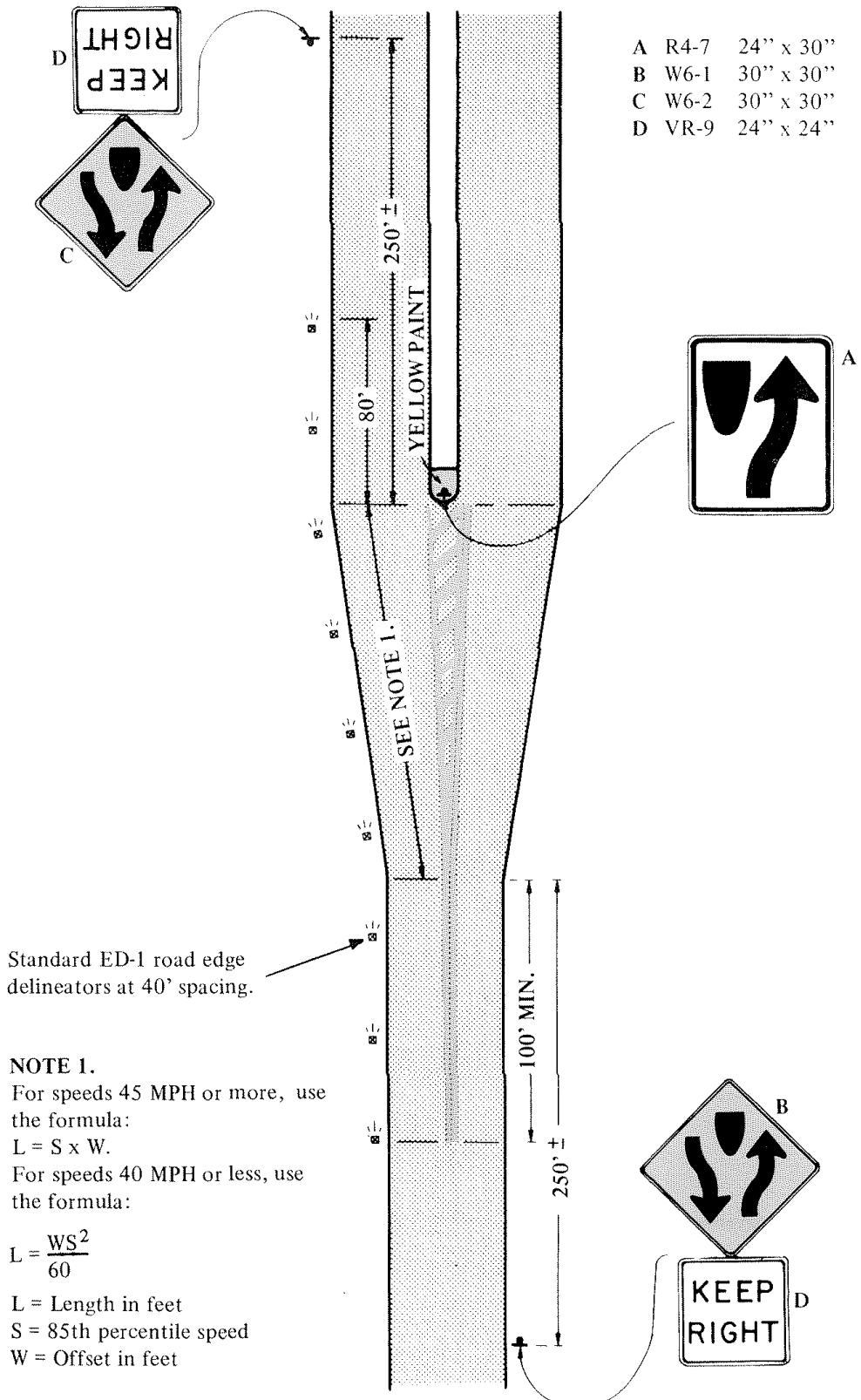
TYPICAL METHOD OF SPEED ZONE SIGNING



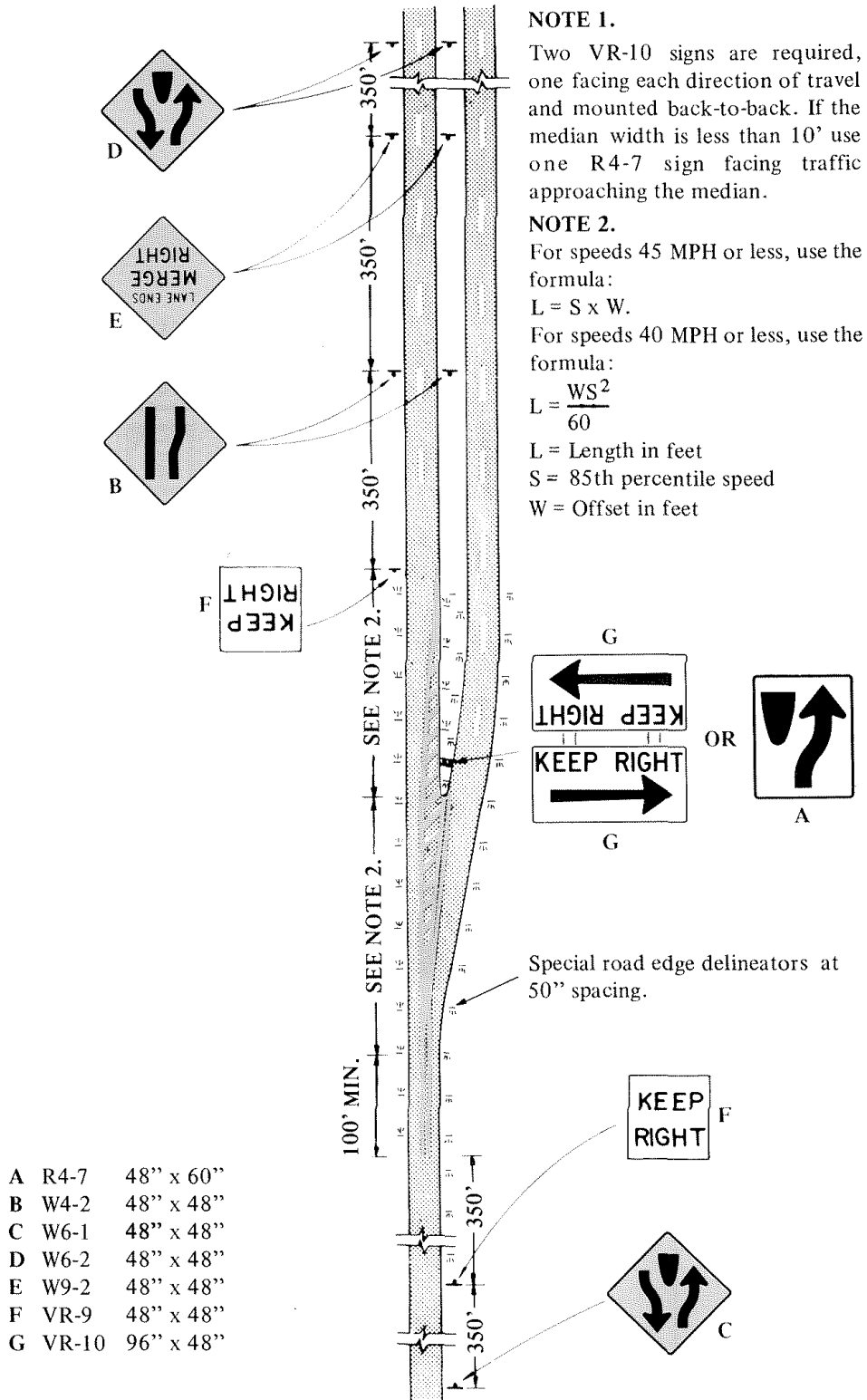
TYPICAL SIGNING OF MOUNTAIN ROAD SECTIONS



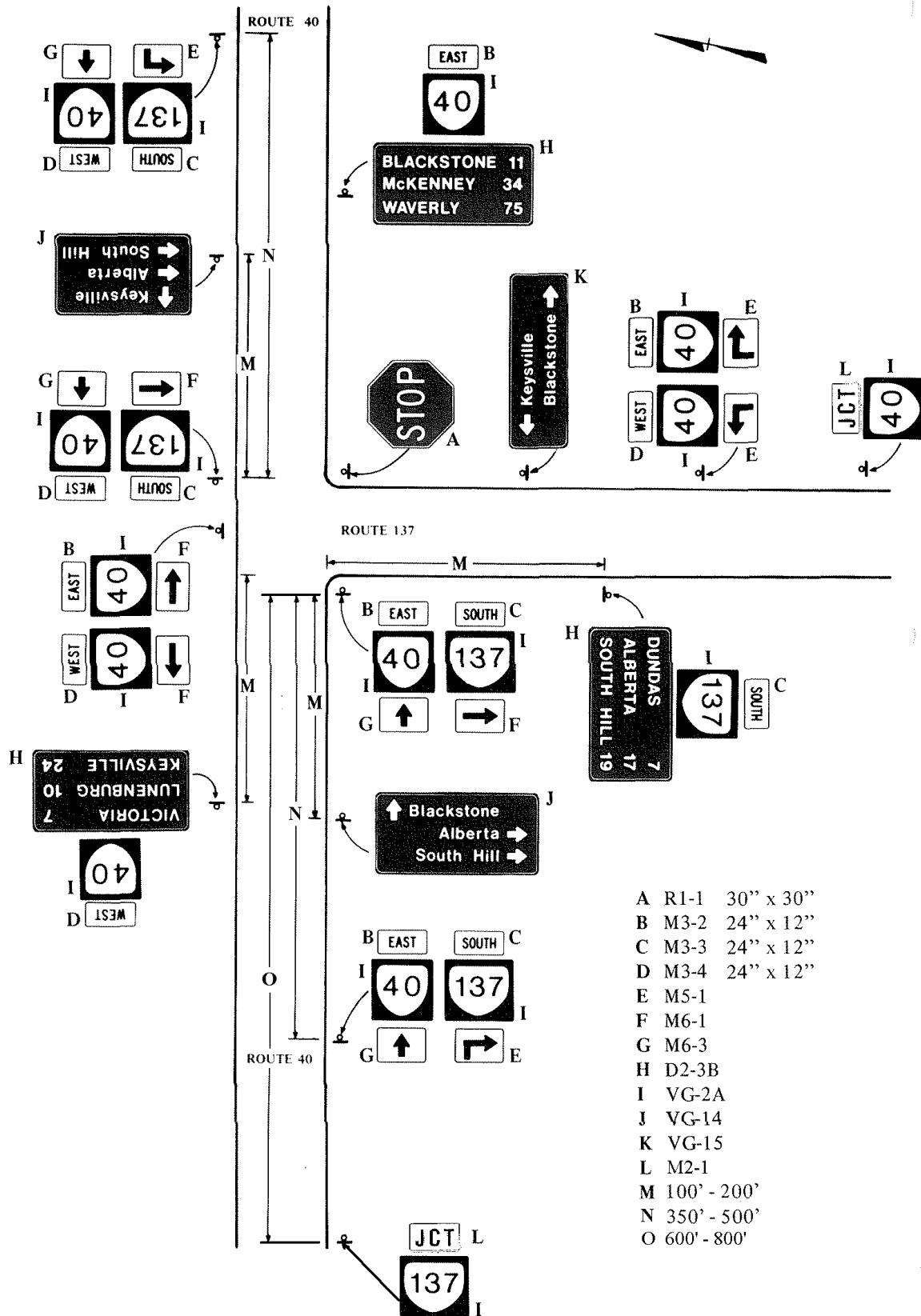
TYPICAL SIGNING AND MARKING FOR TRANSITIONS ON PRIMARY URBAN HIGHWAYS FROM TWO-LANE TO FOUR-LANE DIVIDED.



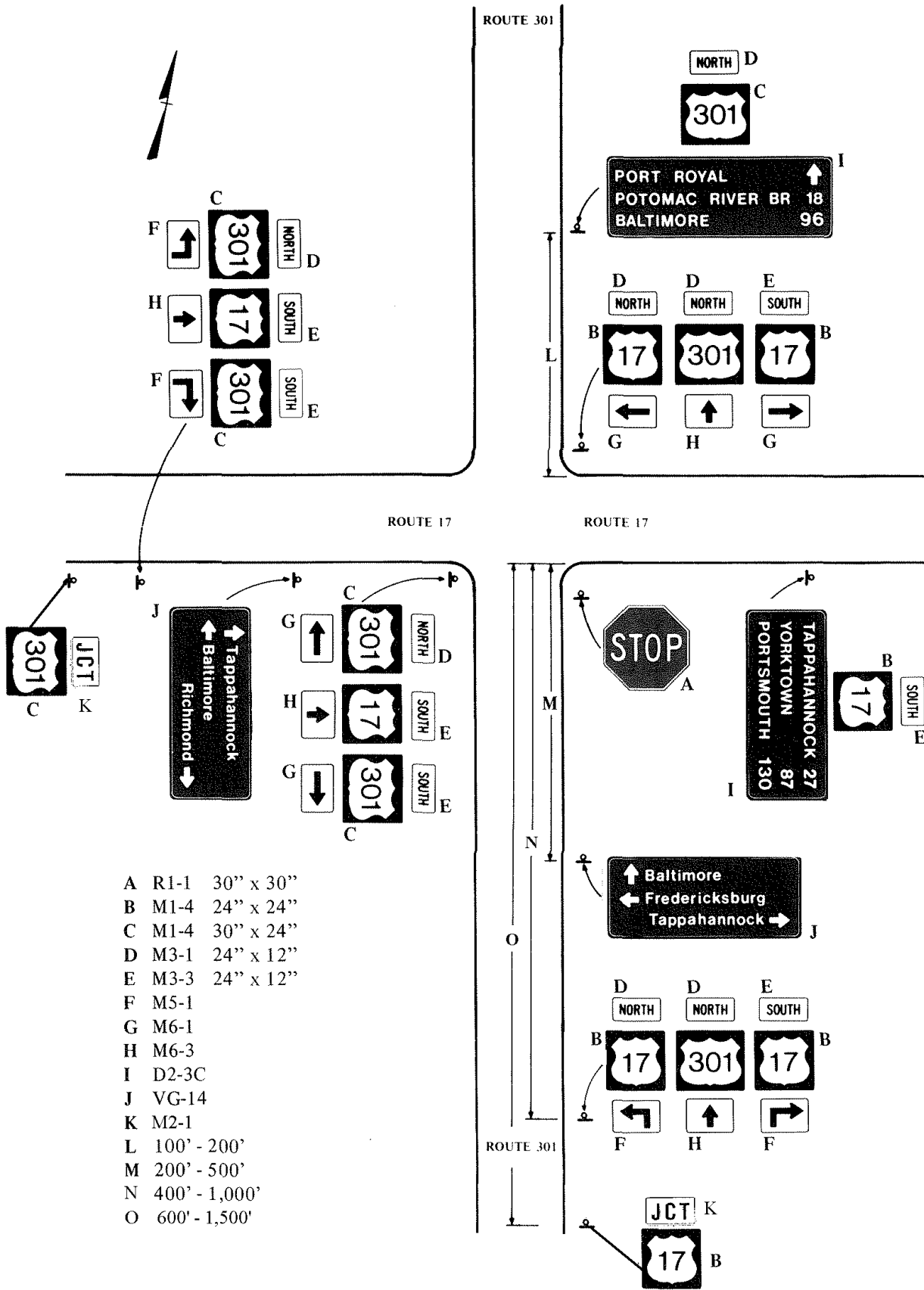
TYPICAL SIGNING AND MARKING FOR TRANSITIONS ON PRIMARY RURAL HIGHWAYS FROM TWO-LANE TO FOUR-LANE DIVIDED.



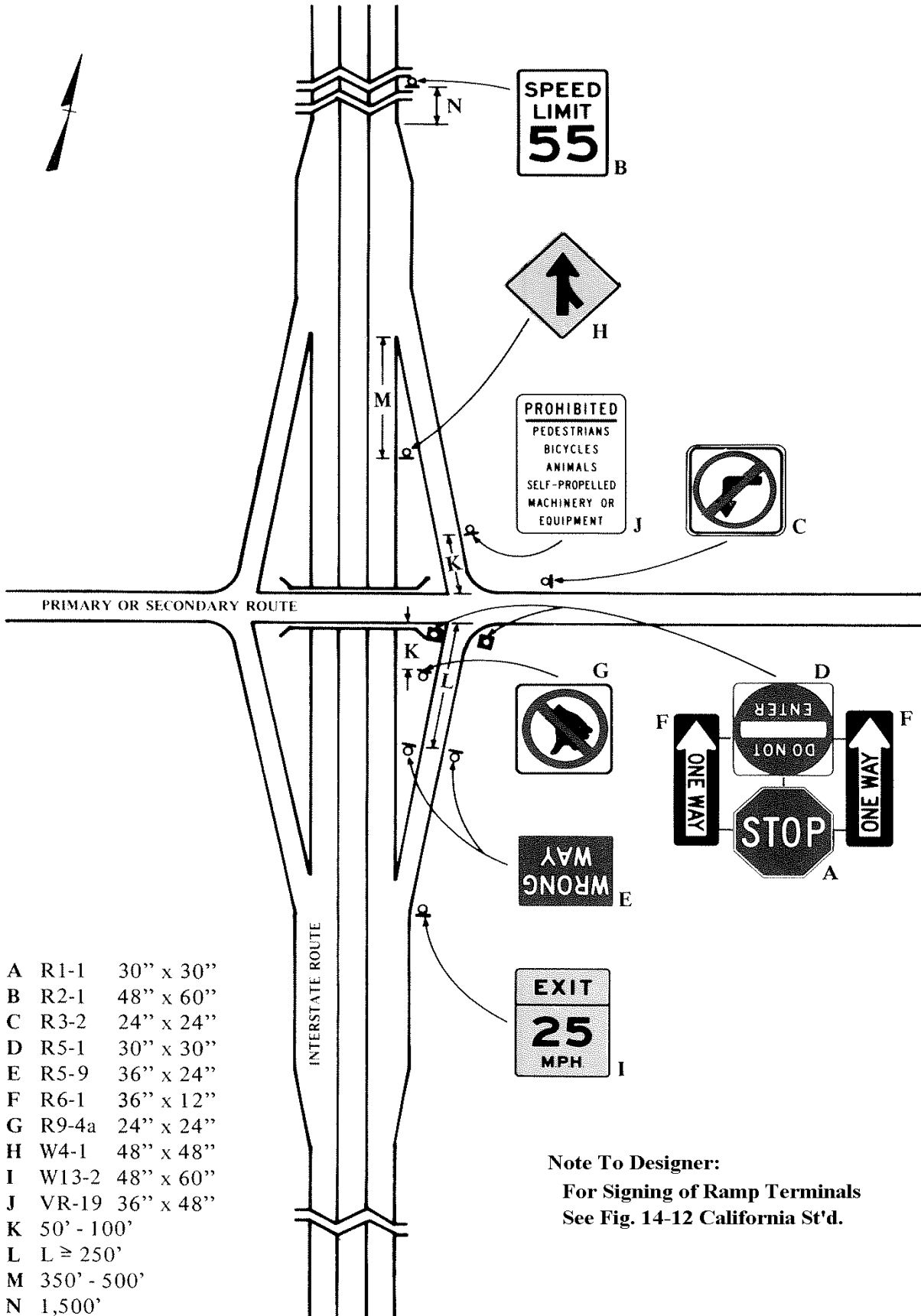
TYPICAL GUIDE SIGN PLACEMENT ON A SMALL TOWN PRIMARY SYSTEM



TYPICAL GUIDE SIGN PLACEMENT ON A RURAL PRIMARY SYSTEM

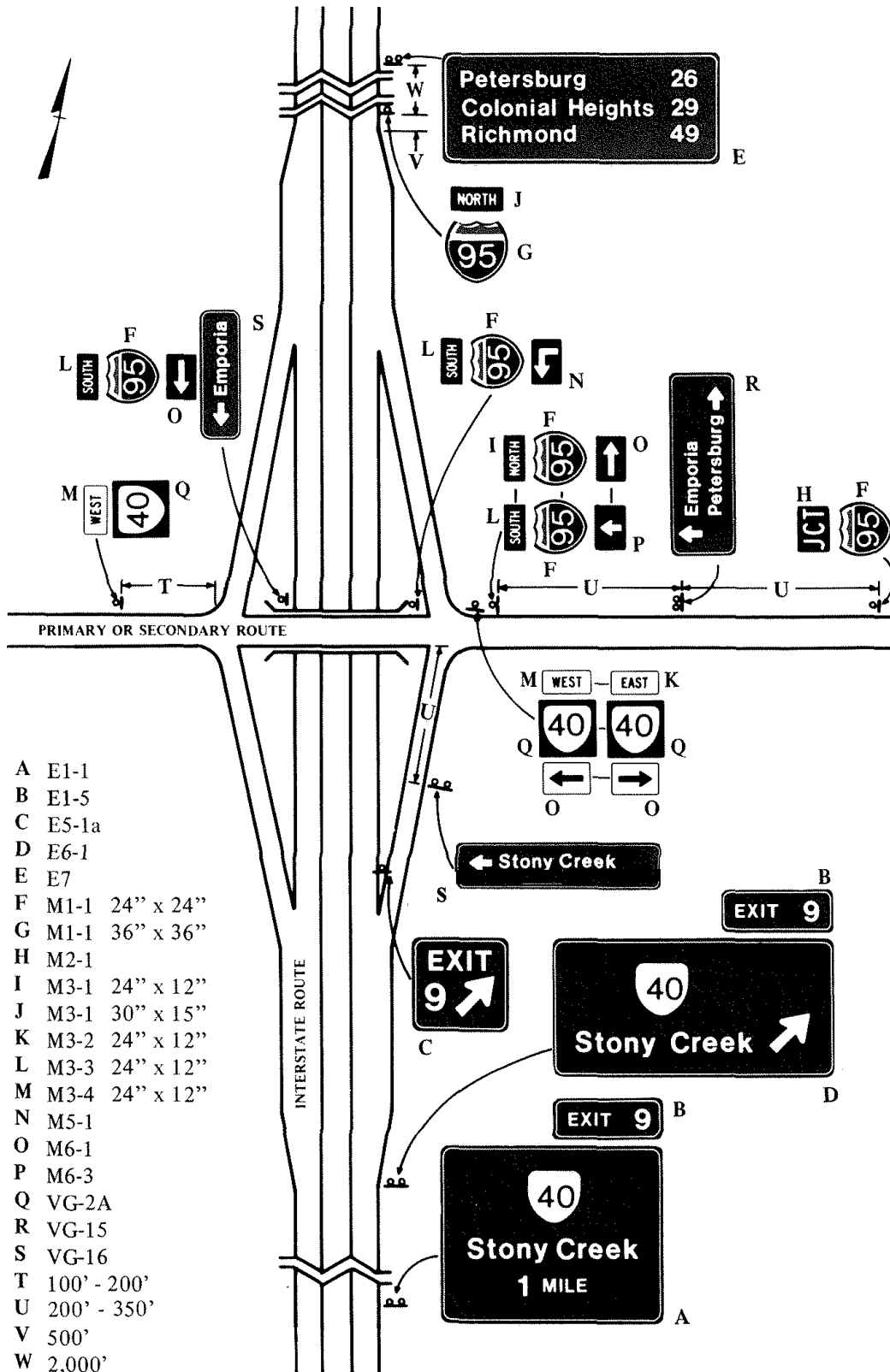


TYPICAL REGULATORY AND WARNING SIGN PLACEMENT ON A DIAMOND INTERCHANGE

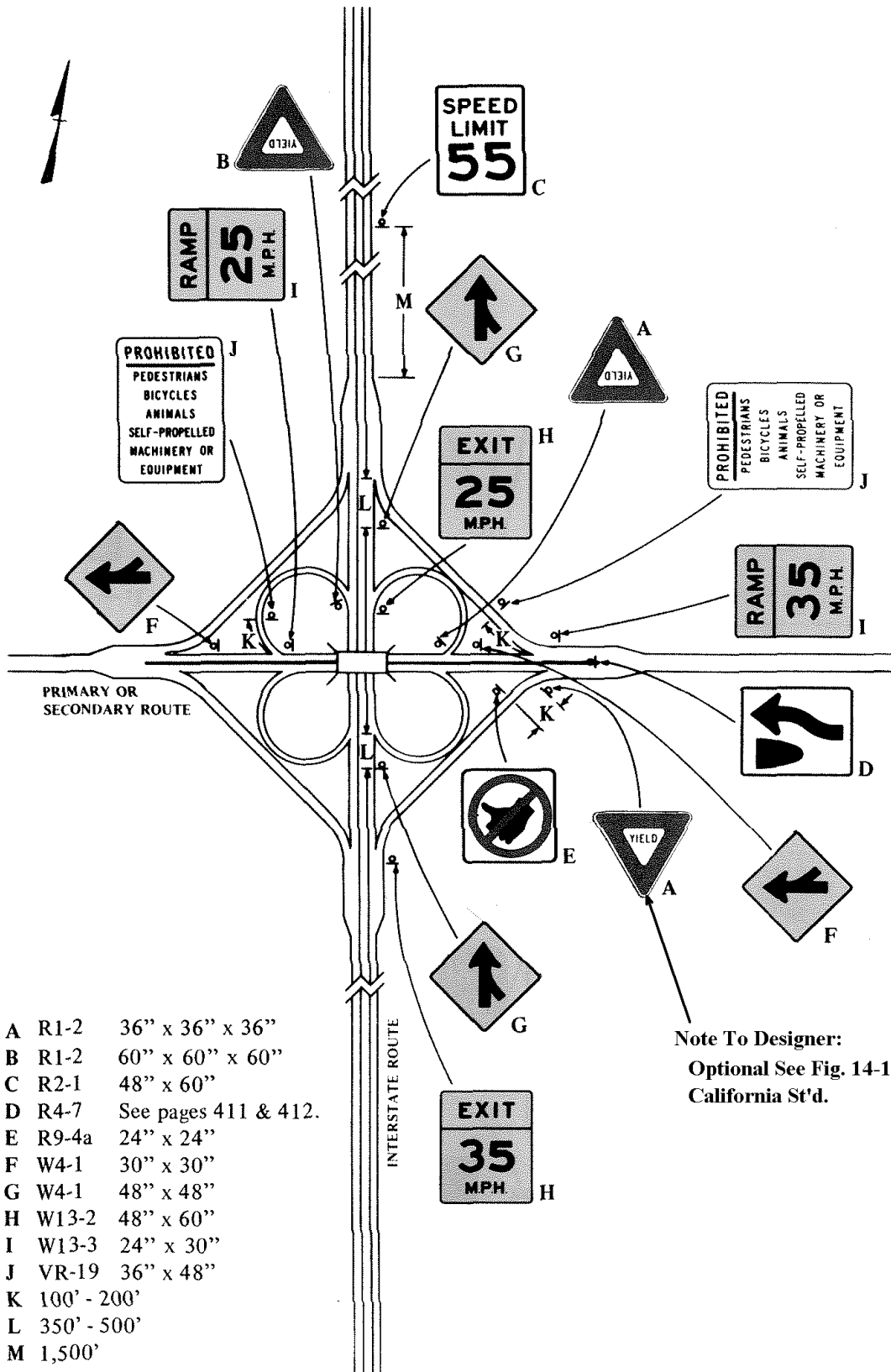


Note To Designer:
 For Signing of Ramp Terminals
 See Fig. 14-12 California St'd.

TYPICAL GUIDE SIGN PLACEMENT ON A DIAMOND INTERCHANGE

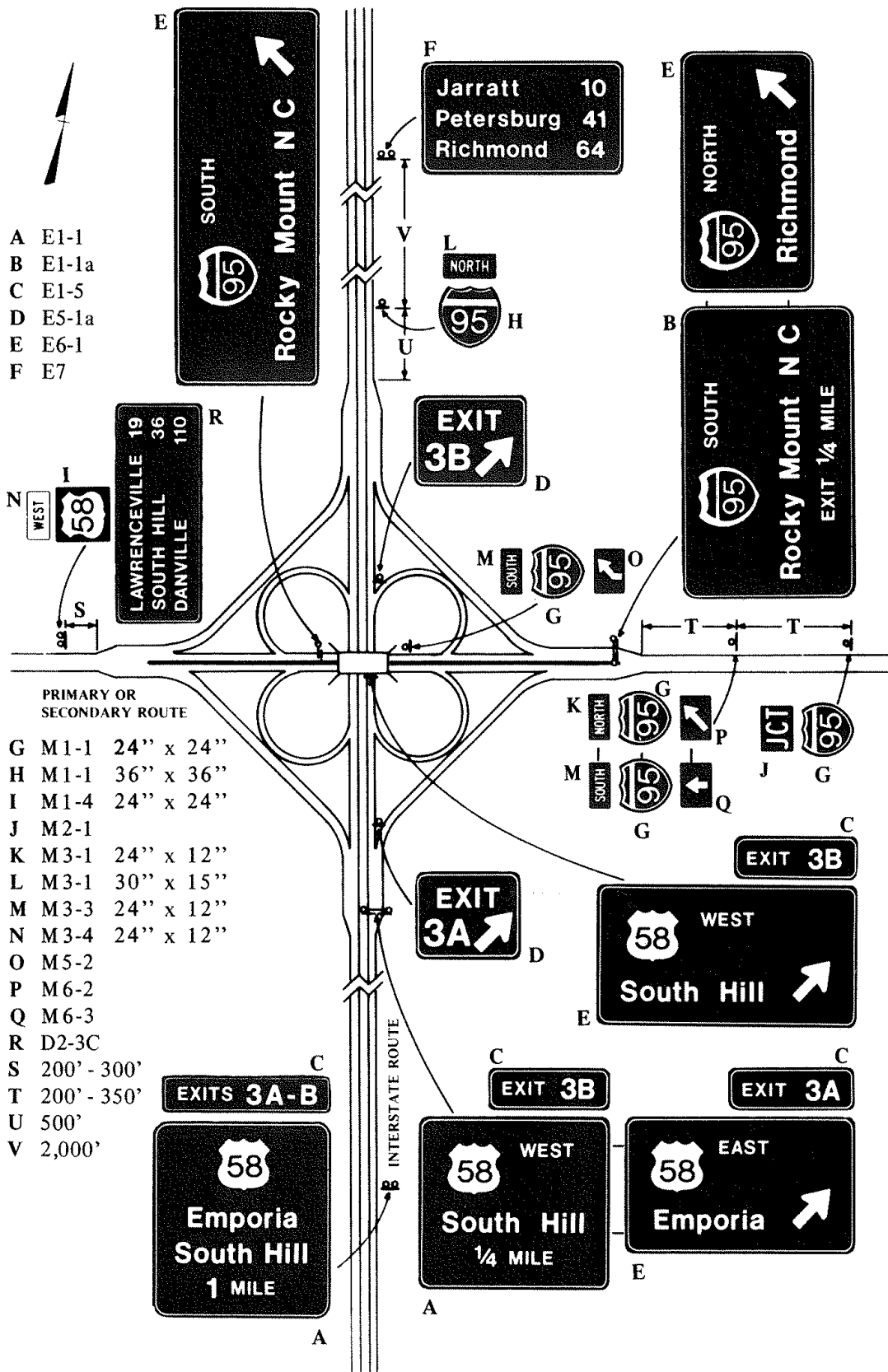


TYPICAL REGULATORY AND WARNING SIGN PLACEMENT ON A CLOVERLEAF INTERCHANGE

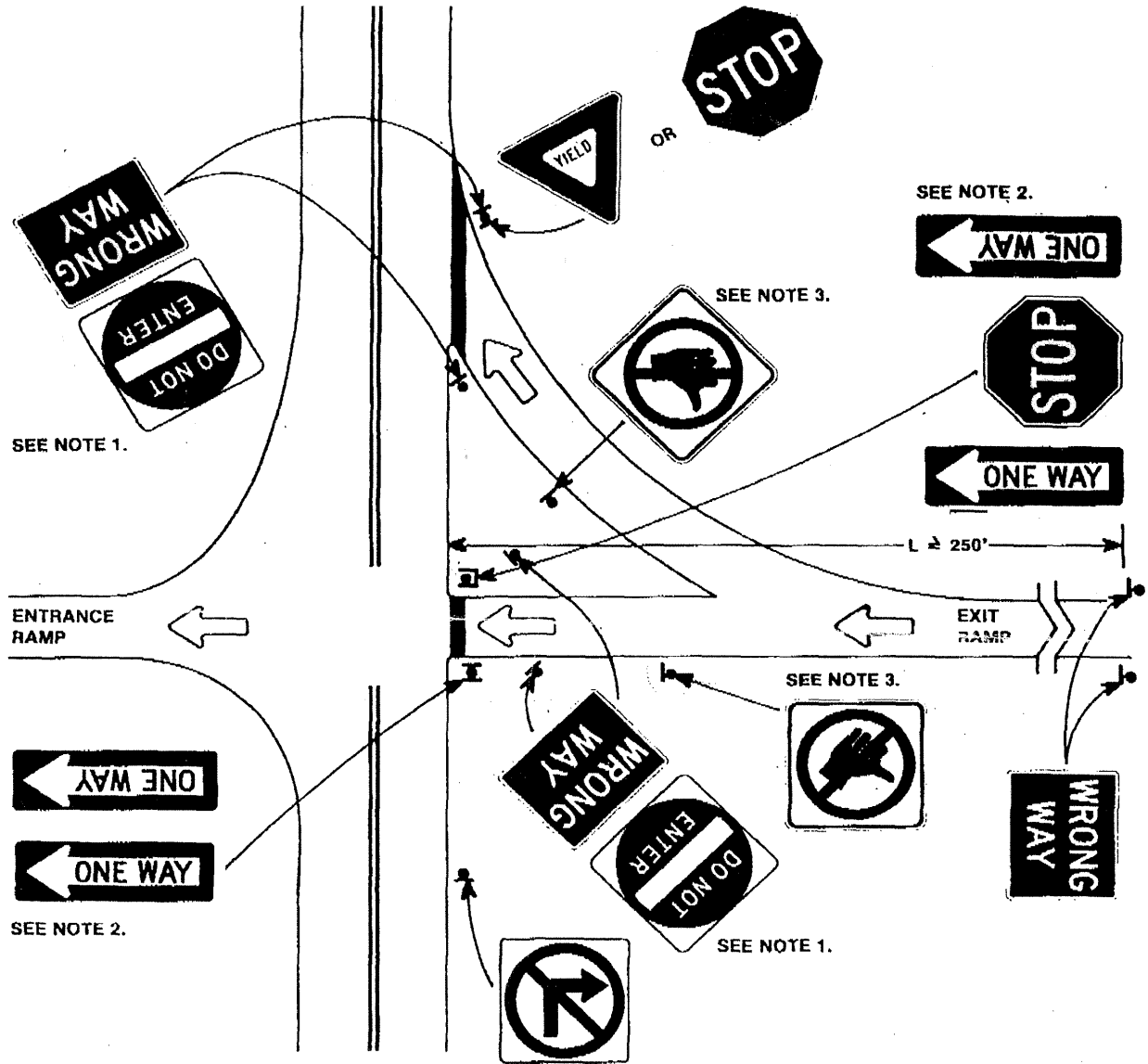


- A R1-2 36" x 36" x 36"
- B R1-2 60" x 60" x 60"
- C R2-1 48" x 60"
- D R4-7 See pages 411 & 412.
- E R9-4a 24" x 24"
- F W4-1 30" x 30"
- G W4-1 48" x 48"
- H W13-2 48" x 60"
- I W13-3 24" x 30"
- J VR-19 36" x 48"
- K 100' - 200'
- L 350' - 500'
- M 1,500'

TYPICAL GUIDE SIGN PLACEMENT ON A CLOVERLEAF INTERCHANGE



CALIFORNIA STANDARD TYPICAL REGULATORY SIGNING FOR EXIT RAMP TERMINALS



NOTES:

1. DO NOT ENTER and WRONG WAY assembly shall be mounted with the bottom of the lower sign 2' above edge of pavement.
2. ONE WAY arrows shall be mounted 3' above edge of pavement.
3. Located 50' - 100' from stop bar.