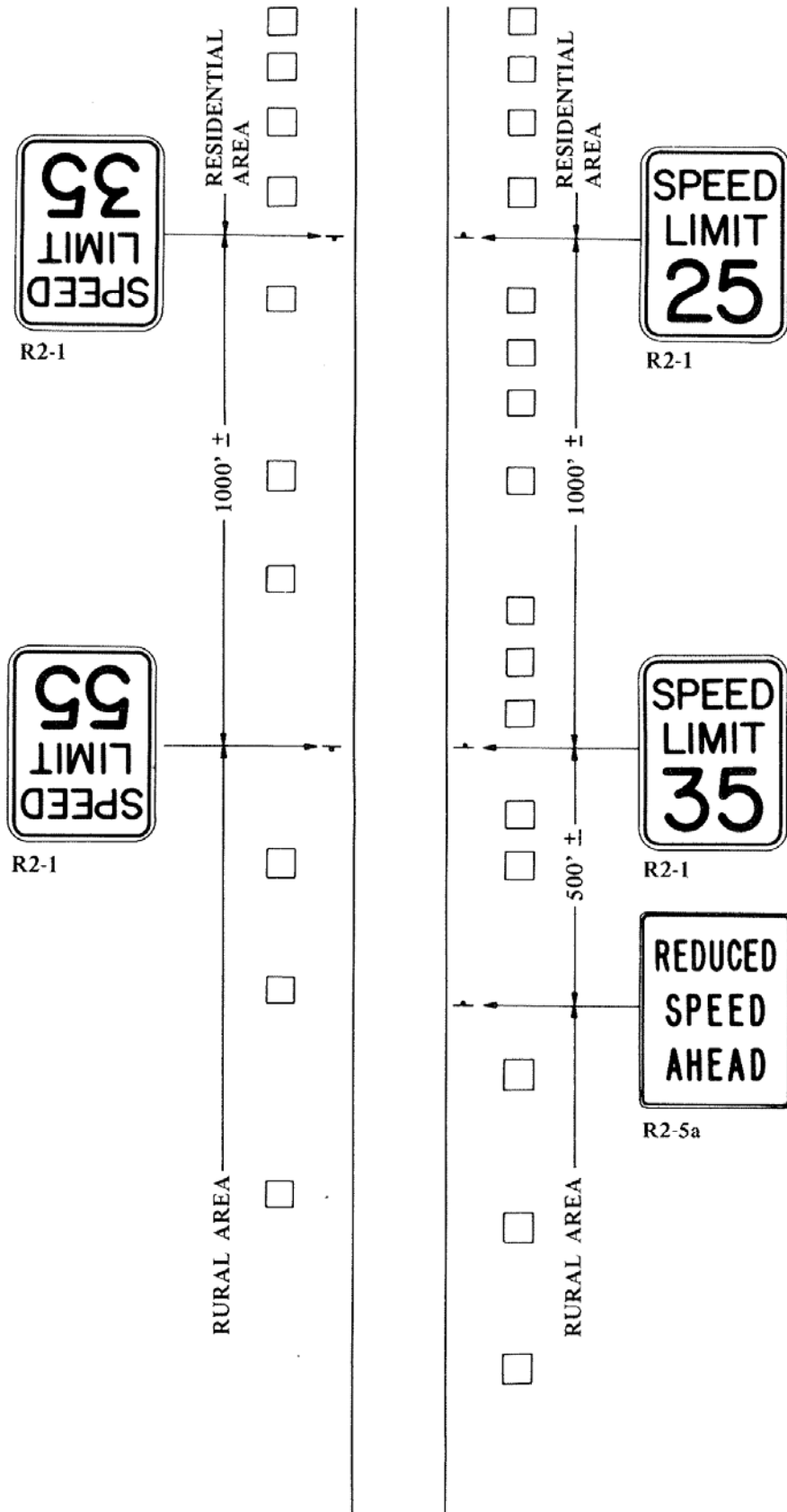
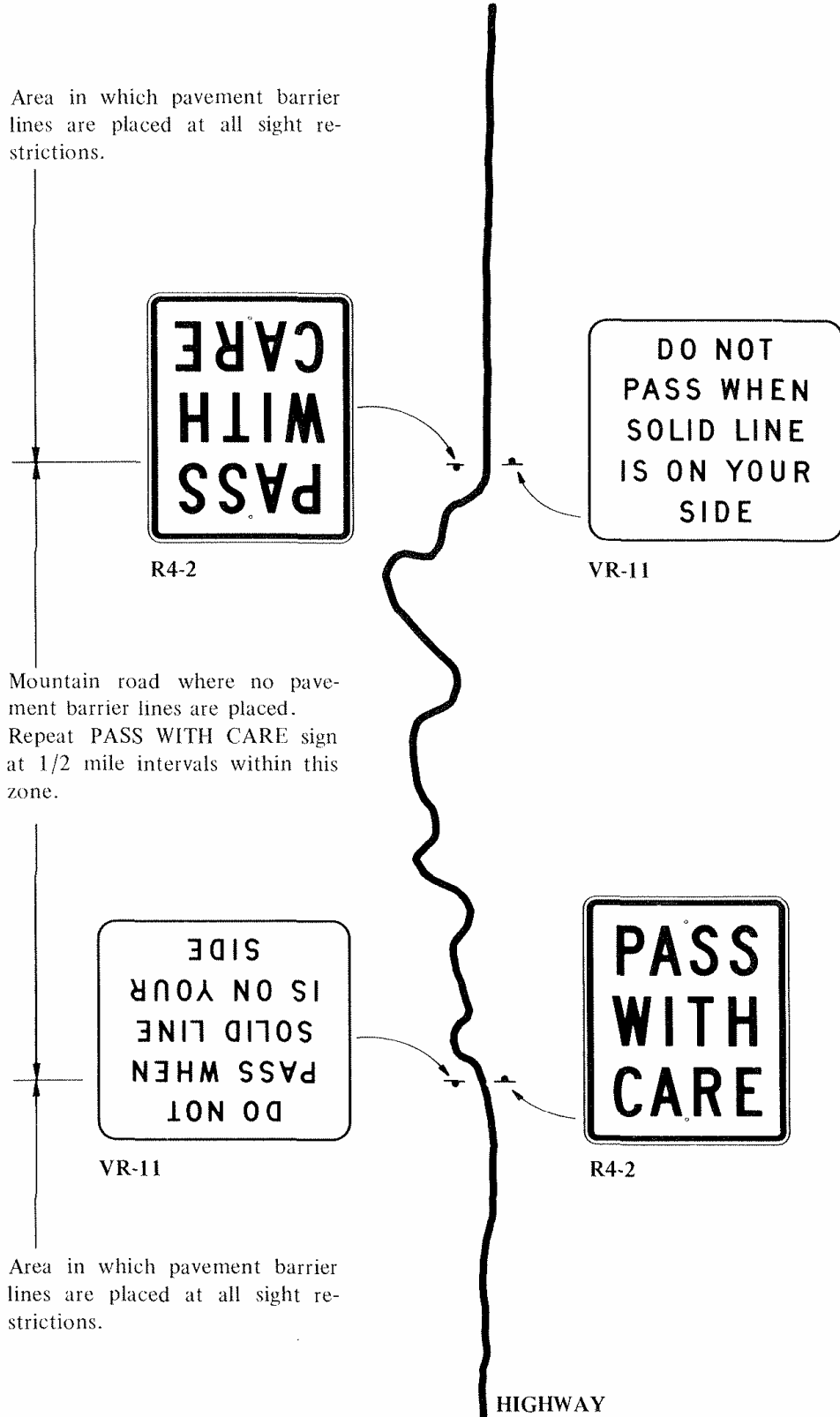




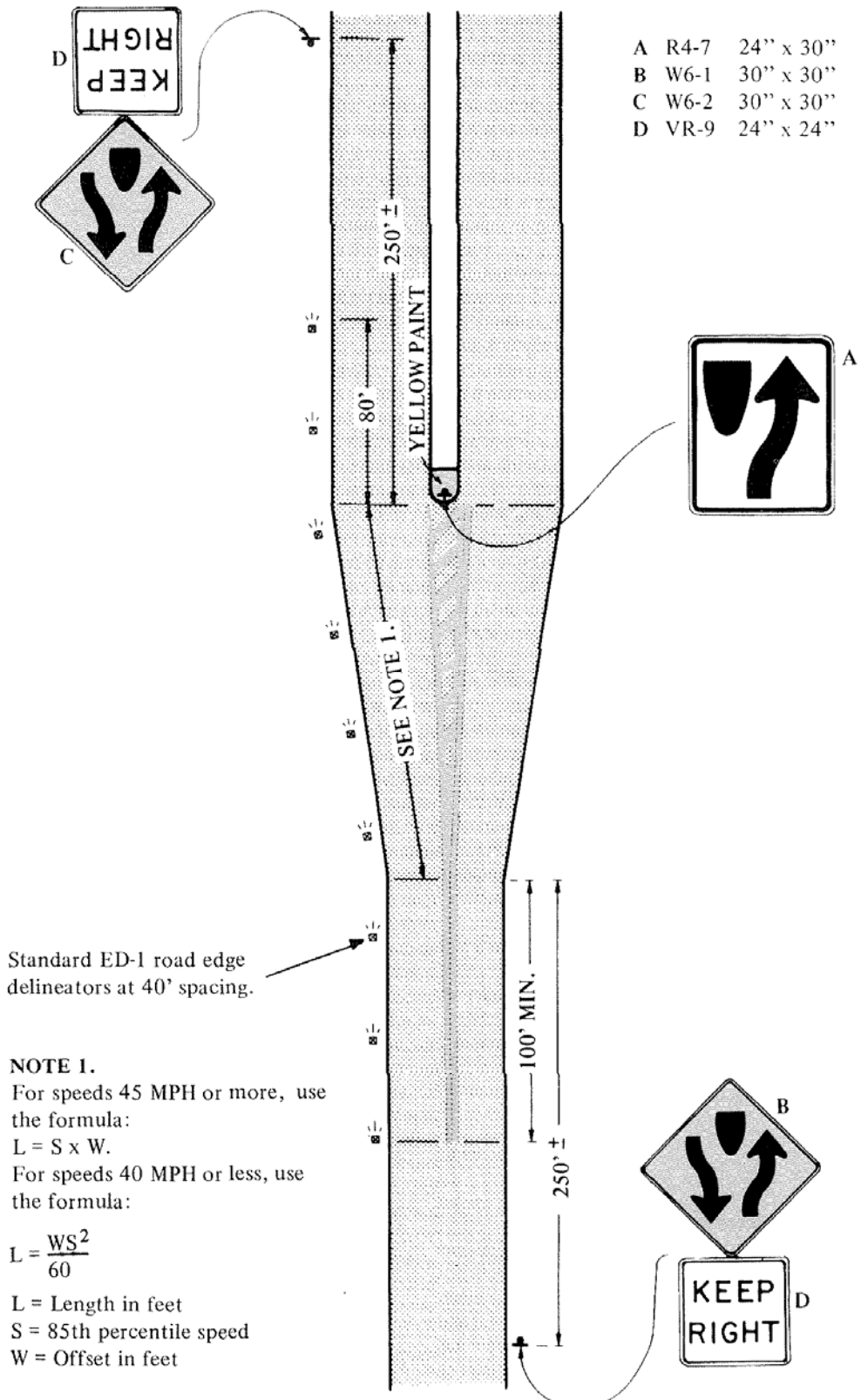
### TYPICAL METHOD OF SPEED ZONE SIGNING



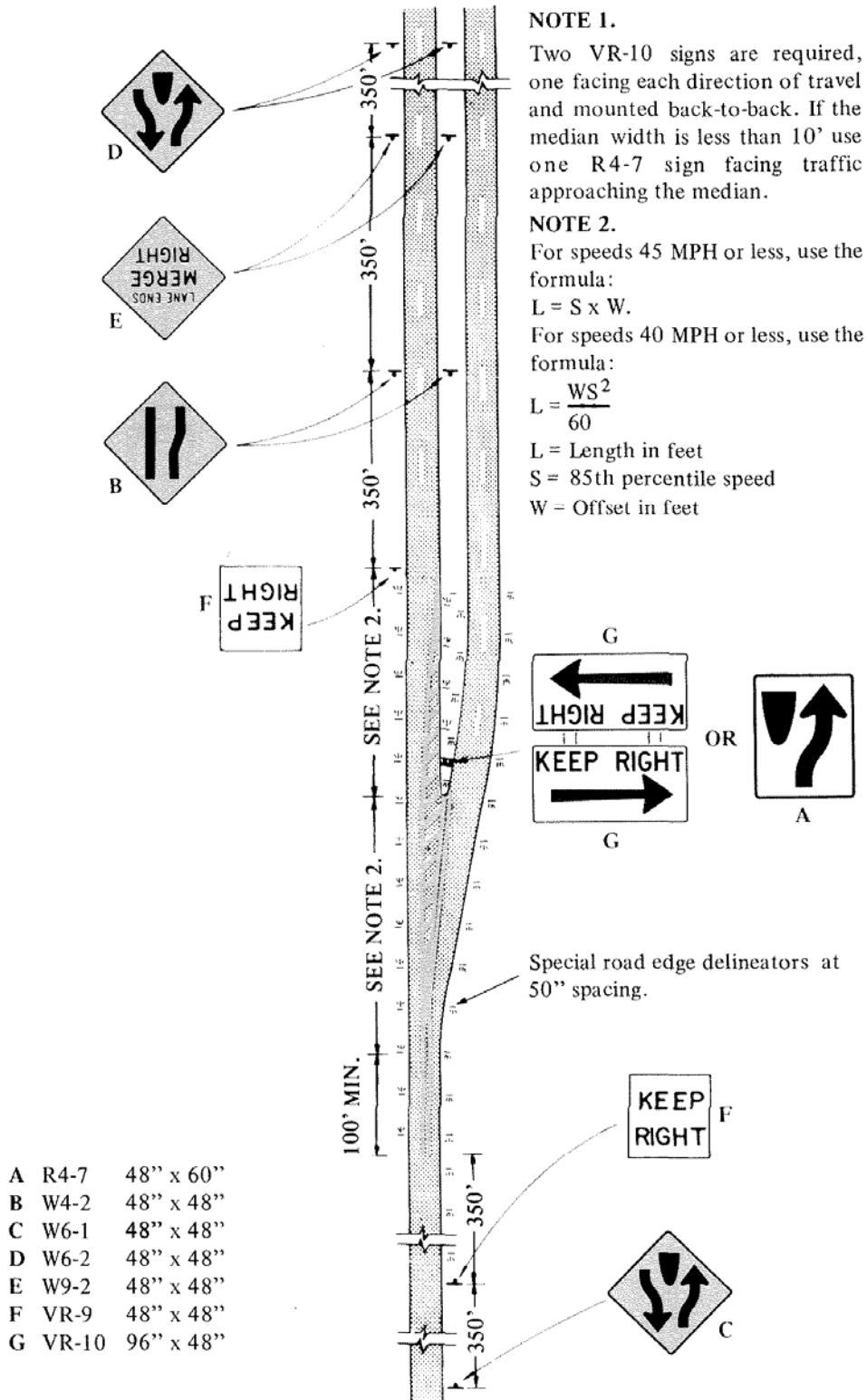
### TYPICAL SIGNING OF MOUNTAIN ROAD SECTIONS



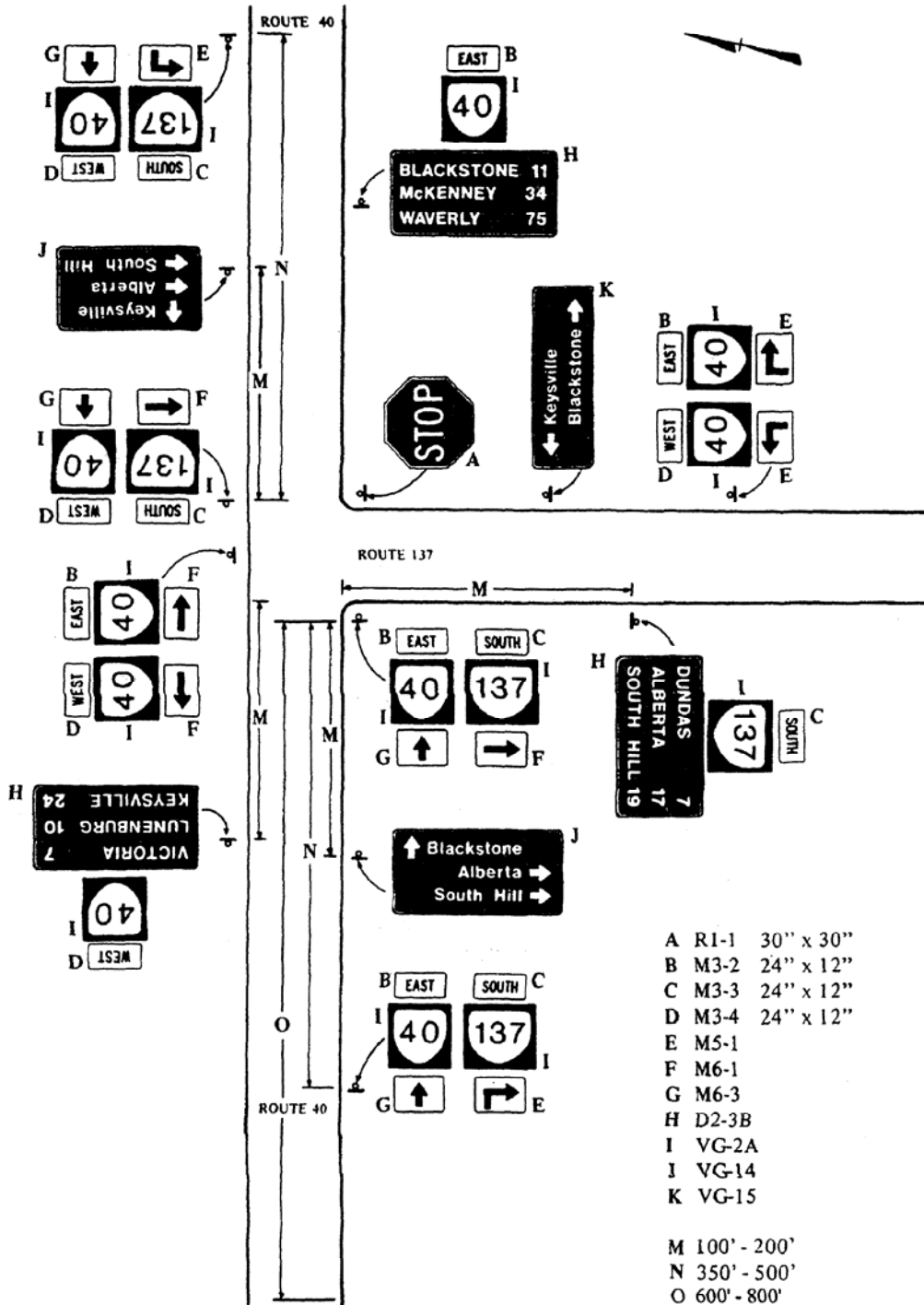
TYPICAL SIGNING AND MARKING FOR TRANSITIONS ON PRIMARY URBAN HIGHWAYS FROM TWO-LANE TO FOUR-LANE DIVIDED.



**TYPICAL SIGNING AND MARKING FOR TRANSITIONS ON PRIMARY RURAL HIGHWAYS FROM TWO-LANE TO FOUR-LANE DIVIDED.**

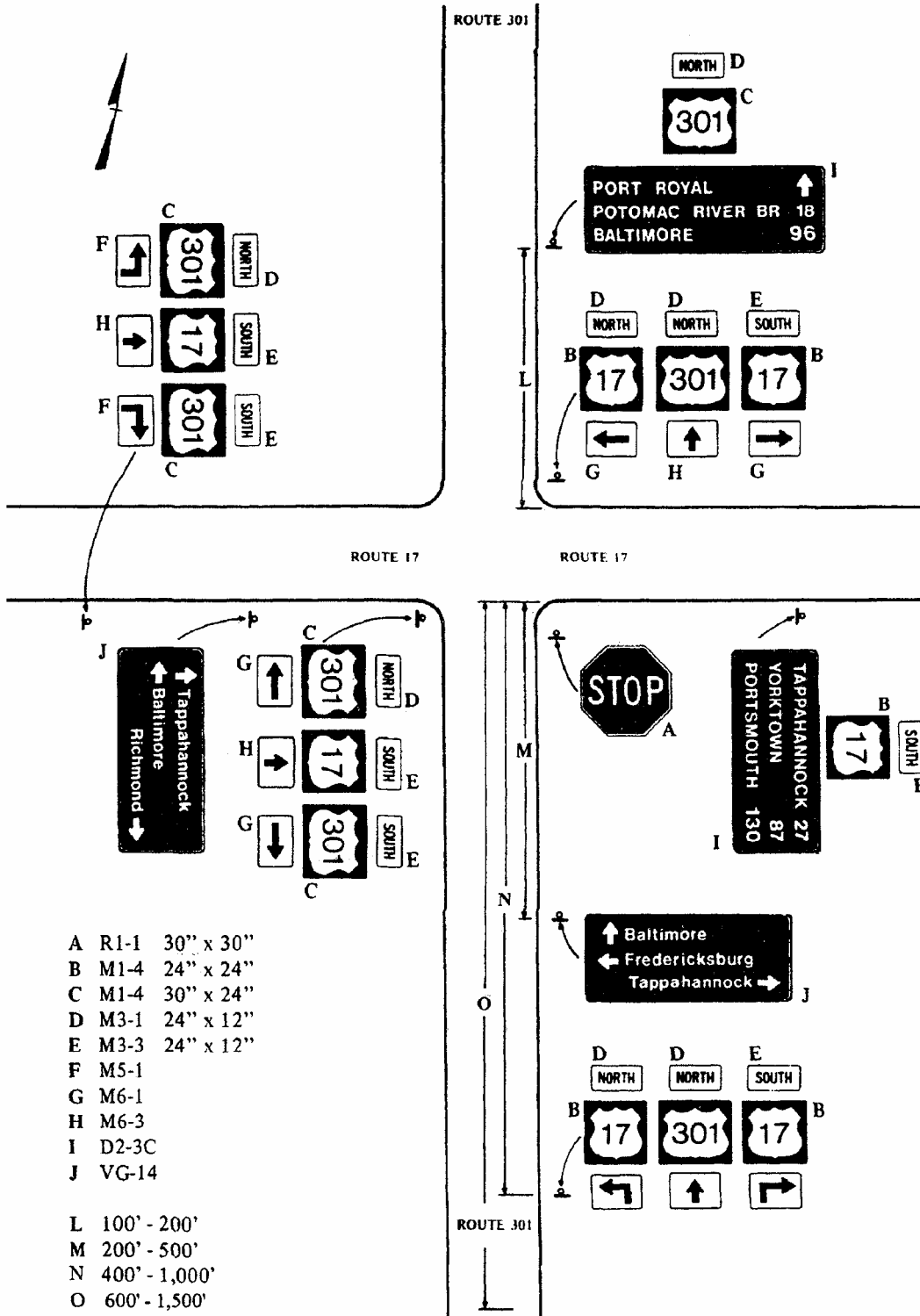


**TYPICAL GUIDE SIGN PLACEMENT ON A SMALL TOWN PRIMARY SYSTEM**



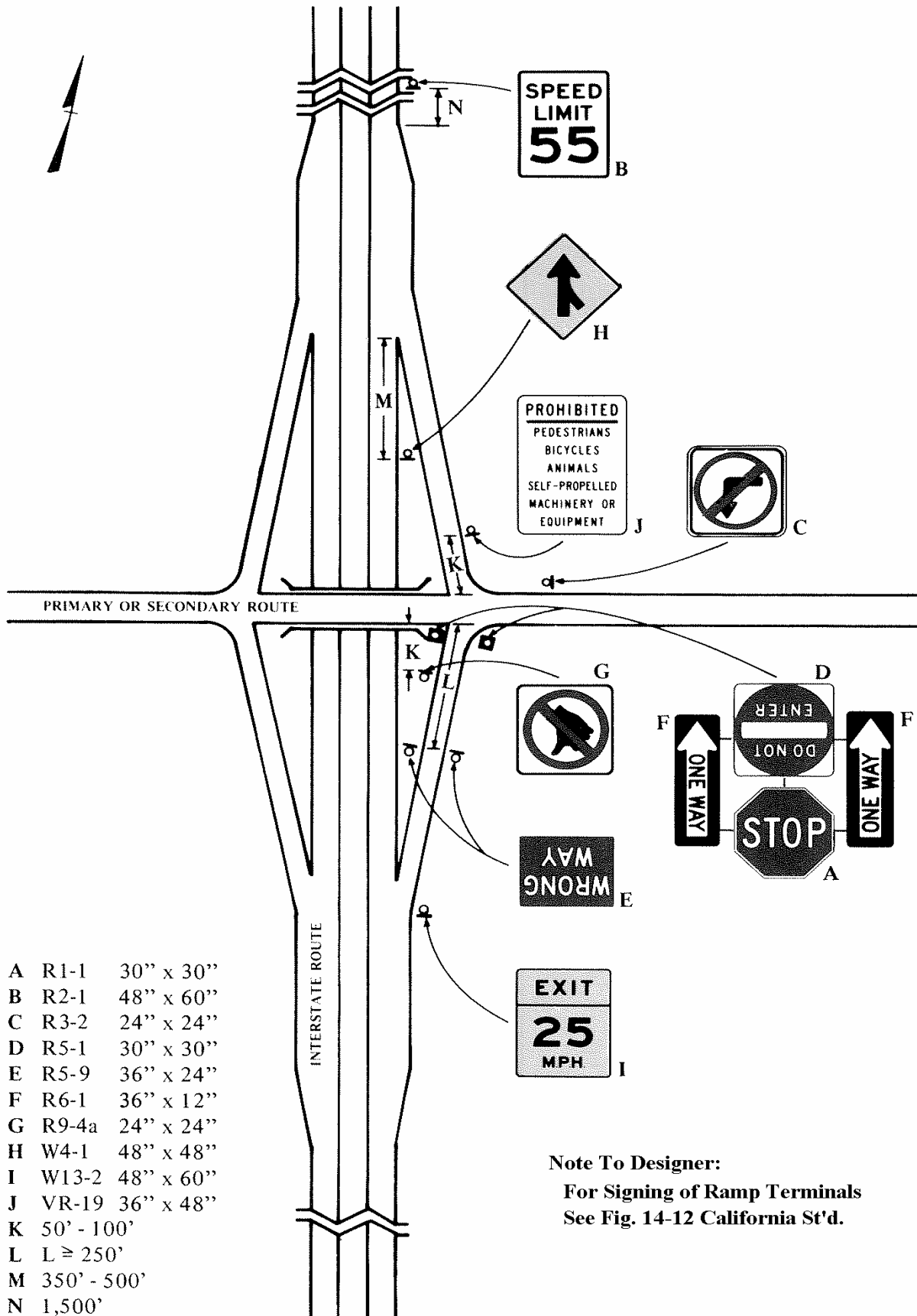
NOTE: The design of Cardinal Direction Message shall be in accordance with the MUTCD.

TYPICAL GUIDE SIGN PLACEMENT ON A RURAL PRIMARY SYSTEM



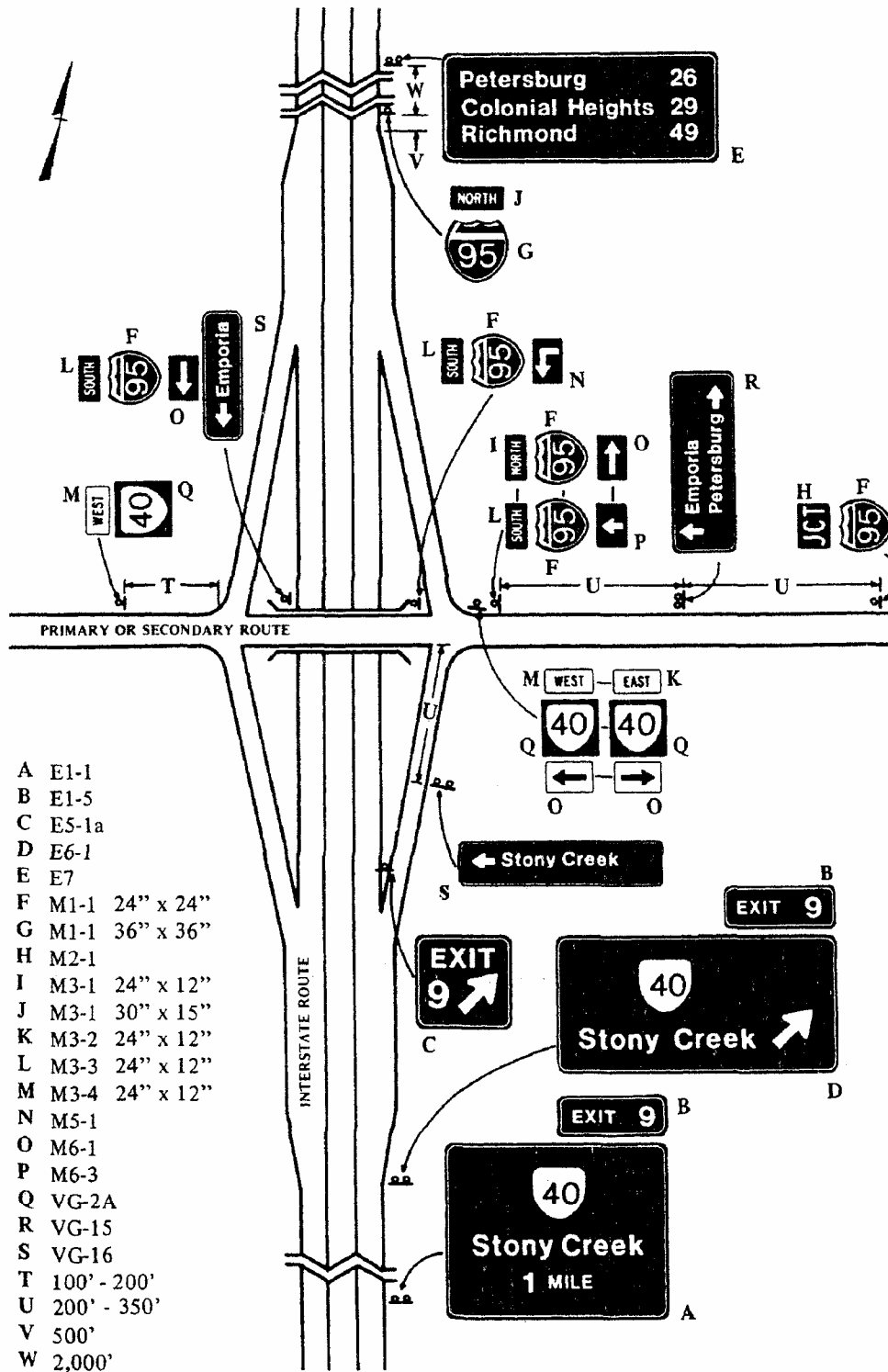
NOTE: The design of Cardinal Direction Message shall be in accordance with the MUTCD.

**TYPICAL REGULATORY AND WARNING SIGN PLACEMENT ON A DIAMOND INTERCHANGE**



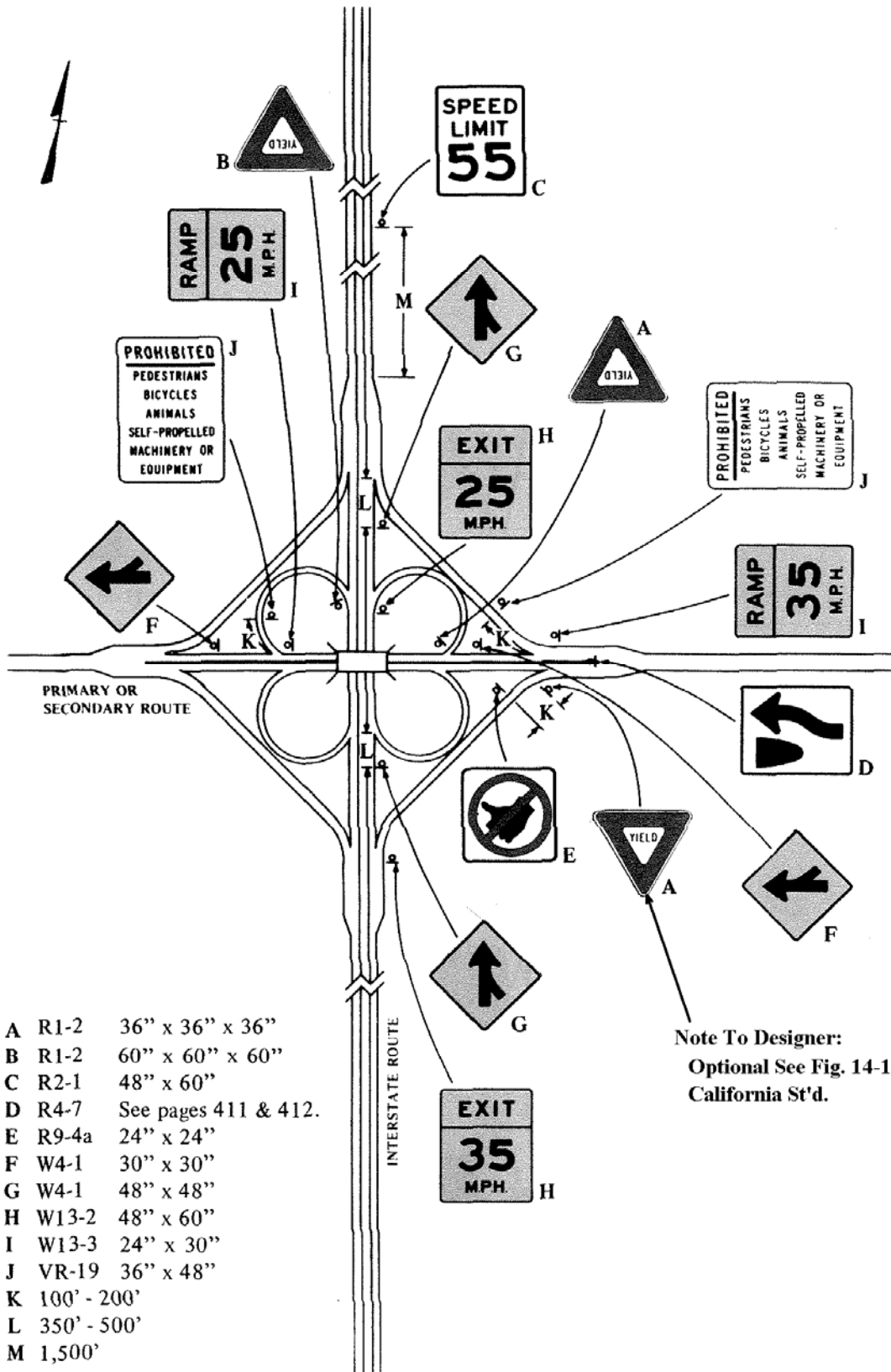


TYPICAL GUIDE SIGN PLACEMENT ON A DIAMOND INTERCHANGE

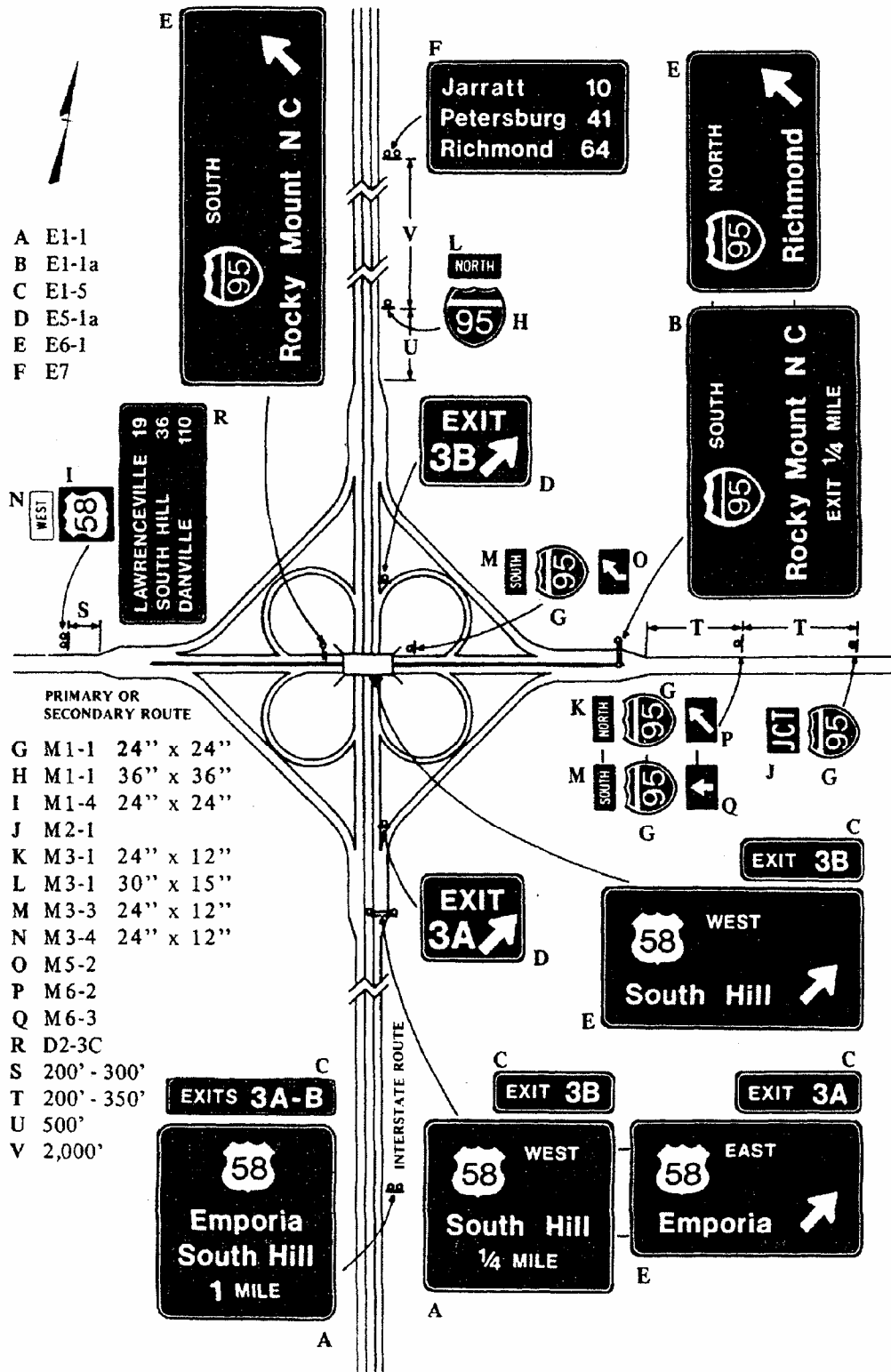


NOTE: The design of Cardinal Direction Message shall be in accordance with the MUTCD.

**TYPICAL REGULATORY AND WARNING SIGN PLACEMENT ON A CLOVERLEAF INTERCHANGE**

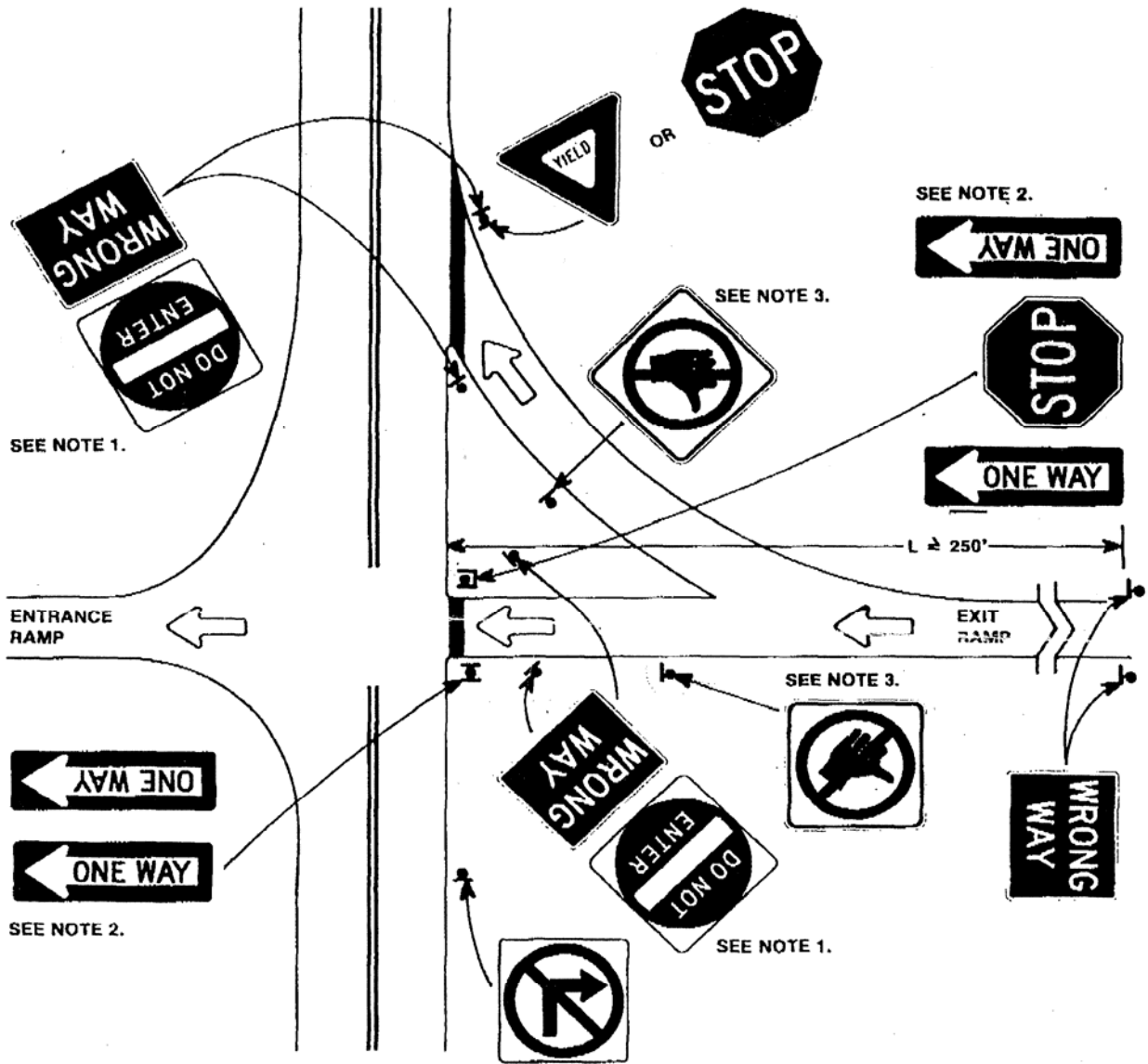


TYPICAL GUIDE SIGN PLACEMENT ON A CLOVERLEAF INTERCHANGE



NOTE: The design of Cardinal Direction Message shall be in accordance with the MUTCD.

### CALIFORNIA STANDARD TYPICAL REGULATORY SIGNING FOR EXIT RAMP TERMINALS



**NOTES:**

1. DO NOT ENTER and WRONG WAY assembly shall be mounted with the bottom of the lower sign 2' above edge of pavement.
2. ONE WAY arrows shall be mounted 3' above edge of pavement.
3. Located 50' - 100' from stop bar.