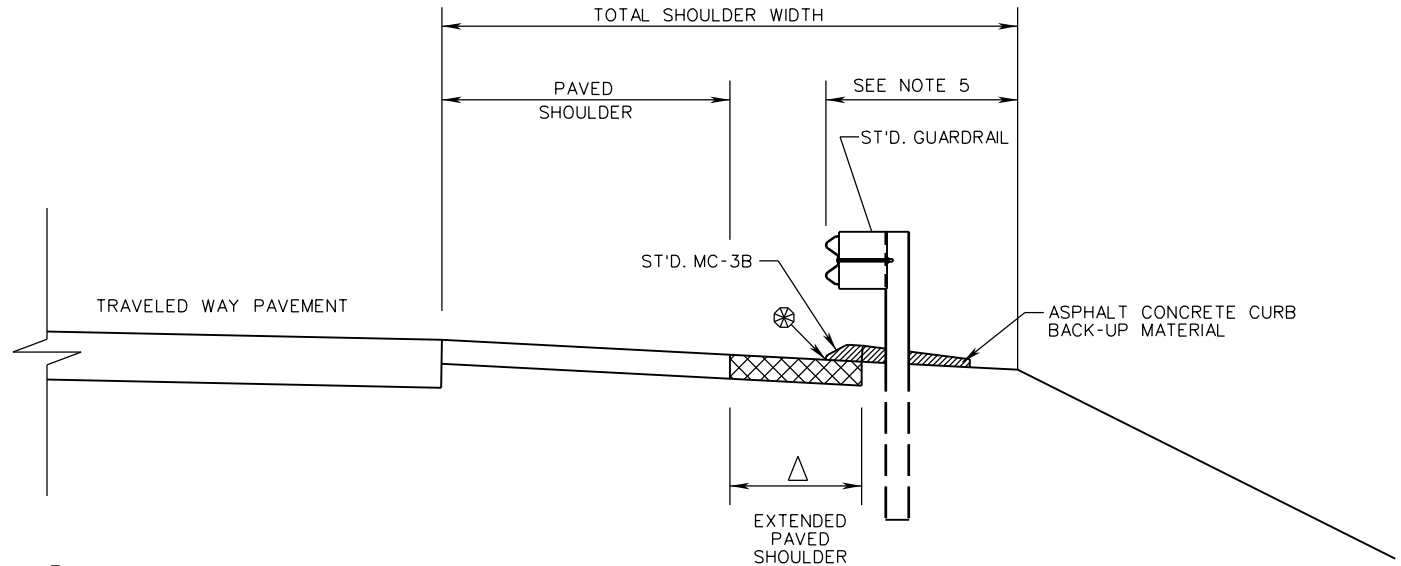


NOTES

1. STANDARD MC-3B REQUIRES THE PAVED SHOULDER TO EXTEND TO THE BACK OF CURB.
2. PAVED SHOULDER WIDTHS TO BE IN ACCORDANCE WITH THE PLANS, VDOT POLICY, OR AS DIRECTED BY THE ENGINEER.
3. THE PAVED SHOULDER AND THE EXTENDED PAVED SHOULDER SHALL BE PLACED SIMULTANEOUSLY.
4. FACE OF GUARDRAIL SHALL BE ALIGNED WITH FACE OF THE CURB.
5. DISTANCE FROM THE FACE OF RAIL TO THE HINGE POINT IN ACCORDANCE WITH THE GUARDRAIL STANDARD USED.
6. MC-3B CURB NOT PERMITTED WITHIN THE LIMITS OF ANY GUARDRAIL TERMINAL.



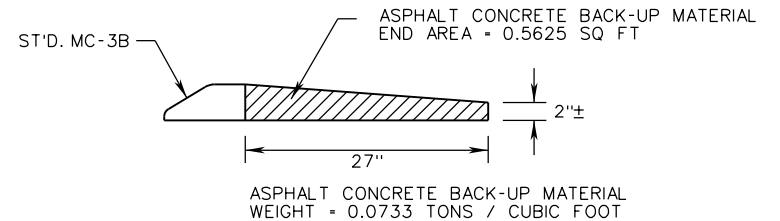
⊗ LIMIT OF SURFACE TREATMENT IF SHOULDER IS TO RECEIVE A PRIME & SEAL. THE PRIME AND SEAL IS TO BE APPLIED TO THE SHOULDER AFTER THE CURB HAS BEEN INSTALLED.

△ TO BE CONSTRUCTED WITH THE SAME MATERIAL AND TO THE SAME DEPTH AS THE PAVED SHOULDER.

STANDARD GUARDRAIL & MC-3B ASPHALT CURB INSTALLATION

TO CALCULATE THE ASPHALT BACKUP MATERIAL

1. MULTIPLY THE LENGTH OF MC-3B BY THE END AREA WHICH RESULTS IN CUBIC FEET.
2. MULTIPLY CUBIC FEET BY 0.0733 TONS / CUBIC FOOT WHICH RESULTS IN TONS OF ASPHALT CONCRETE BACKUP MATERIAL.



SPECIFICATION REFERENCE
105 502

A COPY OF THE ORIGINAL SEALED AND SIGNED DRAWING IS ON FILE IN THE CENTRAL OFFICE.

ASPHALT CONCRETE CURB
(ASPHALT BACKUP MATERIAL INSTALLATION)

VIRGINIA DEPARTMENT OF TRANSPORTATION

VDOT ROAD AND BRIDGE STANDARDS	
REVISION DATE 08/17	SHEET 2 OF 2 201.07